

Manual Amendment

These amendments have been authorised to the text of MotorSport Manual Edition 35. It is recommended that in addition to retaining a copy of this amendment that all parties modify the actual text in their Manual according to the following:

Appendix Two, Schedule A – Driver and Vehicle Safety

Amendments to Schedule A in Manual 35:

1. Amend Part One, Article 2 as stated in the schedule below.
 2. Delete Part One, Article 3.2 as stated in the schedule below.
 3. Amend Part One, Article 3.4 as stated in the schedule below.
 4. Amend Part One, Article 4.1(2) as stated in the schedule below.
 5. Amend Part One, Article 4.7(4)(d) as stated in the schedule below.
 6. Amend Part One, Article 4.8(3)(a) as stated in the schedule below.
 7. Amend Part One, Article 4.11(2)(c) as stated in the schedule below.
 8. Amend Part One, Article 5.3(2)(a) as stated in the schedule below.
 9. Amend Part One, Article 5.6 as stated in the schedule below.
 10. Amend Part One, Article 5.11(4) as stated in the schedule below.
 11. Amend Part One, Article 8.3(6)(d) as stated in the schedule below.
 12. Amend Part Two, Article 4.1(2) as stated in the schedule below.
 13. Amend Part Two, Article 5.5(c) as stated in the schedule below.
 14. Amend Part Two, Article 5.6(b) as stated in the schedule below.
 15. Amend Part Two, Article 5.7(a)(ii) as stated in the schedule below.
 16. Insert new Part Two, Article 5.7(c) as stated in the schedule below.
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1. **Item One:**

2. **Interpretations:** Terms that are specific to this Schedule are defined here. For further definitions refer to National Sporting Code Article 2(1).

“Sports Racing Car” means an open or closed vehicle with provision for two(2) seats disposed one on either side of the vehicles longitudinal centre-line and designed primarily for competition use, and

Reason:

To align definition with Appendix Six.

2. Item Two:

3.2 Deleted.

Reason:

Article deleted.

3. Item Three:

3.4 MotorSport NZ Vehicle Logbook:

(1) Issue and use of Logbooks: MotorSport NZ issues Vehicle Logbooks on receipt of a completed application form and fee. Logbooks are used to record all relevant information pertaining to a vehicle's competition history.

~~**(2) Types of Vehicle Logbook:**~~

~~**(a) ClubSport:** valid for ClubSport Status Events only.~~

~~**(b) National:** valid for all Events permitted by MotorSport NZ.~~

~~**(c)** Competitors with an existing Vehicle Logbook which has been completed shall apply for a Replacement Logbook. All Replacement Logbooks will be issued as a National Logbook.~~

(2) Requirements: It is recommended that all vehicles have a Logbook, however for the following vehicles a valid Logbook is mandatory:

- (a)** All vehicles competing in a Series, and
- (b)** All Purpose Built and all Dedicated Motorsport Vehicles, and
- (c)** All vehicles requiring a LVV / MotorSport Authority card, and
- (d)** As required by the Event Supplementary Regulations.

(3) Contents: The Logbook shall contain the following information:

- (a)** Vehicle identification (make, model, year of manufacture and chassis number), and
- (b)** Which safety schedule the vehicle complies with, and
- (c)** Current ownership details, and
- (d)** Safety Structure homologation or approval certificate (where applicable), and
- (e)** The competition history of the vehicle, including a record of the following;
 - (i)** All Events entered, and
 - (ii)** Safety and eligibility inspections and irregularities, and
 - (iii)** Scrutineers notations, including any significant accident damage, and
 - (iv)** Details of all applied seals (refer Part One Article 3.7)

(4) Validity: The logbook shall be deemed invalid if:

- (a)** The vehicle is found to differ from the identification data, and/or
- (b)** The ¾ view photograph is missing or misrepresentative of the vehicle, and/or
- (c)** The Logbook has been completed and there is no space to make further entries.

- (5) **Entries or amendments to the Vehicle Identification Details:** These may only be made by a MotorSport NZ Technical Officer, or the MotorSport NZ Technical Department.
- (6) **Presentation of Logbook:** Every time a vehicle is used in competition it's logbook shall be presented at documentation. If a logbook is required under Article 3.4(2) above and that vehicles logbook is not available for presentation, for whatever reason, the Clerk of the Course may permit the vehicle to compete if;
- (a) The vehicle is subject to a Safety Audit and clearance is obtained, and
- (b) The penalty as prescribed in Appendix One Schedule P to the National Sporting Code is applied.
- (7) **Entries in logbook:** At documentation, the Chief Scrutineer, or their delegate, shall:
- (a) Check the Logbook for accuracy, previous notations, in particular any items that require rectification, and when the vehicle was last audited, and
- (b) Notate the Event the vehicle has entered and whether the vehicle is being 'Audited' or 'Not Audited'.
- (8) **After an accident:** When significant vehicle damage is sustained, it is the Competitor's responsibility, to present the vehicle and Logbook, to the Event Chief Scrutineer for inspection and notation of any applicable information or as requested by the Clerk of the Course.

Reason:

To revert to one type of logbook.

4. Item Four:

4.1 Protective Helmets:

- (2) **Approved Helmet Standards:** Examples of the following standards labels are detailed in Part Two Article 4 of this Schedule.
- **FIA**
FIA 8860-2004* ††
FIA 8860-2010* ††
 - **Snell Foundation Inc;**
SA 2010* †
SAH 2010* ††
SA 2005* †
SA 2000* †
SA 95 (not valid after 1 January 2016)
M 2010
M2005
M2000
M 95 (not valid after 1 January 2016)
CMR/CMS 2007 (Youth Helmet)
K2005
 - **SFI Foundation Inc;**
SFI Spec 31.1A*
SFI spec 31.2A*
SFI Spec 31.1/2005*
SFI Spec 41.1/2005
SFI Spec 41.1A

SFI Spec 41.2A

- **British Standards Institute;**
BS6658-85 type A/FR (red label), including all amendments * † (not valid after 1 January 2016).
BS6658-85 type A (blue label) including all amendments (not valid after 1 January 2016)
 - **European Standard (R22);**
'E' Mark 04 or 05 series
~~'E' Mark 02 or 03 series (not valid after 1 June 2012)~~
- Note:** *The series number is not the number in the circle. It is the number located close by, an example being; 05-12345.*
- **Australian Standard;**
AS 1698

Notes: * denotes helmets also approved for use in International status Events.
† denotes helmets also approved for use with Forward Head Restraints (FHR) requiring the retro-fitment of tether posts.
†† denotes helmets fitted with tether posts by the helmet manufacturer / agent

Reason:

To correct omission.

5. Item Five:

4.7 Seating:

(4) Seat Installation and Mountings:

- (a) Original seats may retain their original mounts and fasteners.
- (b) Adjustable (fore/aft) seat rails are authorised although for Replacement Seats and Competition Seats, a secure locking method is required on both rails.
- (c) All Replacement and Competition Seats shall be mounted to seat brackets using at least four(4) (two(2) in the front and two(2) in the rear), self locking ISO 8.8 M8 bolts.
- (d) All Seat brackets shall mount directly to the bodyshell/chassis or fabricated frame in four(4) locations (front right/left and rear right/left) using at least four(4) self locking ISO 8.8 M8 bolts. **Seat brackets shall have a minimum thickness of 3mm for steel or 5mm for aluminium.**
- (e) The Seat brackets and/or fabricated frame shall:
 - (i) Mount directly to the vehicle manufacturers' original seat mounting points, or
 - (ii) Alternative mounting points may be fabricated, each incorporating a reinforcement plate measuring at least 40cm² x 3mm, or
 - (iii) Transverse members incorporating inserts used in combination with reinforcement plates measuring at least 40cm² x 3mm which may be welded or bolted to the bodyshell. These members may attach directly to the Safety Cage in which case they must be detailed on the Safety Cage homologation. Where one(1) of these members terminates, either on the Safety Cage or at a reinforcement plate, welding shall be carried out throughout the entire perimeter of the member.

Reason:

To correct omission.

6. Item Six:

4.8 Fire extinguishers:

(3) Mounting / Location: Extinguishers shall be installed in accordance with the following:

- (a)** For hand-held extinguishers, a metal retaining system incorporating a quick release metal strap/s shall be fitted (a minimum of two(2) metal straps shall be required from 1 January 2013), secured to the structure of the vehicle by a minimum of two(2) self locking ISO 8.8 M6 bolts with panel washers. All hand-held extinguishers shall be positioned within easy reach of the occupant/s while in their normally seated position.
- (b)** For mounted (plumbed in) systems, each storage cylinder shall be installed with a minimum of two(2) metal straps and four(4) self locking ISO 8.8 M6 bolts with panel washers or in accordance with the manufacturers mounting instructions. The occupant(s) shall be able to trigger the extinguisher system while in their normally seated position. For closed vehicles, an external triggering device located at the base of the windscreen may also be fitted, and
- (c)** Extinguishers shall be mounted so the gauge is visible at all times.

Reason:

To define the grade and size of mounting bolts.

7. Item Seven:

4.11 Steering and Suspension Systems:

(2) Requirements: These systems shall:

- (a)** Provide a smooth steering action without tightness, roughness or excessive freeplay, and
- (b)** Have no components that are loose, excessively worn, or damaged, and
- (c)** Have the steering column lock disabled or removed; mandatory for Dedicated Motorsport Vehicles and optional for all other vehicles.

Reason:

To correct omission.

8. Item Eight:

5.3 Service Fluid Lines:

(2) Lines Located in the Cockpit: All lines may be routed through the cockpit although this is not recommended for lubricating oils and/or coolants. The following conditions apply:

- (a)** Fuel and brake lines shall be appropriately rated, be metal (pure copper tube is not authorised for brake lines) or have a metal outer braiding and all joins shall be of a threaded or crimped industry quality type; and

Reason:

To differentiate the lines relative to the non-authorised material.

9. Item Nine:

5.6 Lighting Systems:

- (1) **Brake lights:** shall be fitted to all vehicles (except Single Seater and Sports racing cars) and shall be:
- (a) Maintained in good working order, and
 - (b) Of at least 21watts (each lamp) or equivalent LED.
- (2) **Rear Lights / Rain Lights:** shall be fitted as follows:
- (a) For all circuit events all vehicles shall be fitted with either their fully operational Series Production rear lights in good working order, or rear light/s that are;
 - (i) At least 15watts (each lamp), or equivalent LED, that maintain constant illumination (unless detailed otherwise in a current Sanctioned Series Technical Schedule), and
 - (ii) Clearly visible from the rear, and
 - (iii) Robust enough to operate throughout any event entered, and
 - (iv) Mounted within 100mm of the vehicles centre-line (except where more than one(1) lamp is fitted), and
 - (v) Able to be switched on by the Driver in their seated position.

Reason:

The current bulb reference is not a readily available bulb and to permit flashing rain lights.

10. Item Ten:

5.11 Windows: shall meet the following requirements;

- (4) **Plastic windows:** are authorised under the following conditions:
- (a) Material thickness shall be at least three(3) mm for side windows and 4.5mm for either front or rear windscreens.
 - (b) The front windscreen shall have at least one(1) internal vertical metal support strap, and
 - (c) The rear window shall have at least two(2) external vertical metal support straps, unless the material thickness exceeds five(5) mm, and
 - (d) The metal support straps shall be at least 3mm x 25mm and be securely fastened to the bodyshell at each end, and
 - (e) The side windows shall be fitted as per their original mounting or adequately secured to the vehicle.

Note: *Additional requirements apply to road registered vehicles, refer Part One Article 8.3*

Reason:

To align material specifications with the (readily available) industry standard and to remove the requirement of straps on rear windows of more than five mm thickness.

11. Item Eleven:

8.3 LVV / MotorSport Authority Card: All (motorsport) vehicles used on public roads that have any (or all) of the dedicated modifications as detailed in Part One Article 8.3(4) below are required by law to hold an Authority Card. The Authority Card system for motorsport vehicles is accepted in Law under the Land Transport Compliance Rule and is administered by MotorSport NZ under the Low Volume Vehicle Code. The Authority Card is a certification document, issued by MotorSport NZ that specifies the alternative safety related equipment required by a vehicle, for purposes of motor sport competition.

(6) Specific Requirements:

- (d) Removal of a Manufacturer Occupant Protection System (MOPS):** from a vehicle (primarily used for motor sport competitions) may be authorised under the Authority Card system. The vehicle shall be fitted with and comply with the following:
- (i)** A minimum of a 4 strap safety harness in full compliance with this Schedule shall be fitted to both front seating positions, and
 - (ii)** A minimum Safety Cage as per diagrams 8.3(6)(d) below incorporating forward reinforcements (terminating at a substantial structure such as the suspension strut tower) in full compliance with this Schedule, and
 - (iii)** Competition seats, compliant to an FIA standard (8855-92 or 8855-99) and in full compliance with this Schedule, and
 - (iv)** The airbag system shall be entirely removed including its operating system, all triggering sensors, and warning lamps, and
 - (v)** The steering wheel shall be replaced by a conventional direct replacement steering wheel that requires no modification to the steering column shaft to permit its fitment, or if the original fitment wheel is retained, have the cavities that formerly housed the airbag unit filled with energy absorbing foam, and
 - (vi)** The original manufacturer installed safety belts shall be completely removed.
 - (vii)** A warning notice (available from the MotorSport NZ Administration office) shall be applied in a position that may easily be read by the driver detailing the following: **“Warning – This vehicle is no longer fitted with a manufacturer installed frontal impact protection system.”**

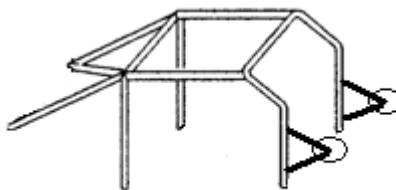


Diagram 8.3(6)(d) – forward reinforcement

Reason:

To remove definition of MOPS as already defined in Article 2.

12. Item Twelve:

4.1 Helmet Standards:

(2) Snell Standards Labels:



Reason:

To correct omission.

13. Item Thirteen:

5.5 Safety Cage Attachment:

- (c) **Mounting foot:** A mounting foot may also be used but is generally only used for dismountable structures. Where used each foot must be attached by at least three(3) bolts for the main and lateral / front rollbar, and at least two(2) bolts for the backstays of minimum ISO 8.8 M8 or be welded directly to the reinforcement plate/footing box. The mounting foot shall be at least 3mm thick steel plate, and shall be smaller than the reinforcement plate/footing box to which it is affixed. (Refer Diagram 5.5(c)).

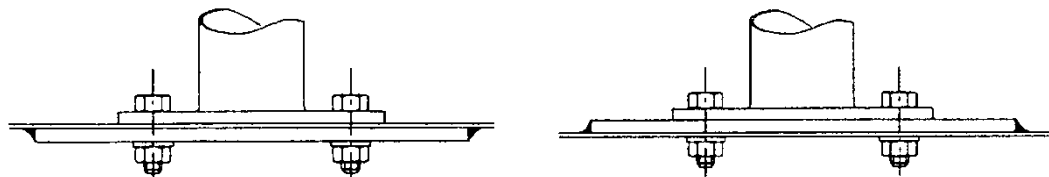


Diagram 5.5(c) – Mounting Foot Attachment

Reason:

To define the grade and size of mounting bolts.

14. Item Fourteen:

5.6 Construction Specification for Single Seater and Sports Racing Cars: Single Seater and Sports Racing Cars with Safety Rollbars/Cages homologated after 1 January 2011 shall comply with the requirements of FIA Appendix J, or the requirements of this Schedule.

(b) Specification for Sports Racing Cars:

- (i) All designs shall incorporate a Main rollbar in compliance with the requirements of Part Two Article 5.4(a), and
- (ii) The Main rollbar shall extend transversely across the full width of the cockpit with the outer most edge extending beyond the Occupant's shoulders when in their normal seated position, and
- (iii) The Main rollbar shall be no further than 250mm rearwards of the normally seated Occupant(s), and
- (iv) For all vehicles two(2) Lateral Bracing stays shall be fitted in accordance with Part Two Article 5.4(c). Where Backstays are used an optional single forward facing brace that extends from a central position on the Main rollbar to the passenger floor area may also be fitted.

For Open Vehicles only, the two(2) Backstays may be replaced by two(2) forward facing Lateral Bracing stays, and

- (v) A Front rollbar is optional and may be incorporated as part of the vehicles frame forward of the steering wheel.

Reason:

To correct error.

15. Item Fifteen:

5.7 Alternative Design: Safety Cages that may differ from the minimum Material and Design specifications of this Schedule shall be classified as 'Alternative Design'.

- (a) **All Vehicles except Single Seater and Sports Racing Cars:** Shall comply with the following: For these calculations it shall be taken that only the forward lateral, main rollbar and backstay footings are rigidly mounted.

- (ii) **Arithmetic static load test on the Front Rollbar:** Where the structure is asymmetrical this load shall be applied to both sides of the vehicle.

0.035 W kN load shall be applied to the top of the front rollbar (at the intersection of the forward lateral and windscreen bar) directed downward, and to the front of the vehicle at an angle of 5° relative to the horizontal, and to the exterior of the vehicle at an angle of 25° relative to the horizontal in accordance with diagram 5.7(a)(ii).

At no point shall the stress achieved in structure exceed the tensile strength of the material, and deformation shall **not** exceed 100mm in the direction of the applied load.

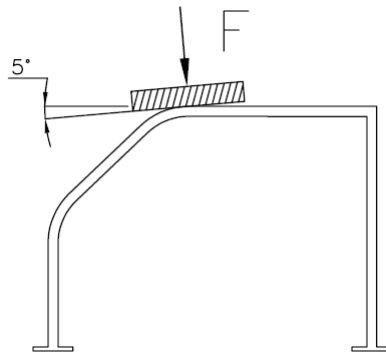


Diagram 5.7(a)(ii)

Reason:

To correct omission.

16. Item Sixteen:

5.7

- (c) **Safety Harness Bar:** If the minimum material dimensions as detailed in the chart under Part Two Article 5.2 are not complied with, the resistance of each anchorage point must be subject to calculation and included in the engineers report. The report must clearly demonstrate that the safety harness bar withstands a static loading of 1470daN without breakage.

Reason:

To include the safety harness bar in alternative design for safety cages.

This amendment is issued for MotorSport New Zealand Inc by the Administration.

B. Budd
General Manager

